

An aerial photograph of a coastal town, likely Wasaga Beach, showing a mix of residential buildings, green spaces, and a waterfront area with a beach and water. The image is overlaid with a semi-transparent white rectangle containing the title text.

DOWNTOWN WASAGA BEACH

URBAN DESIGN GUIDELINES

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1.0 INTRODUCTION

1.1. Purpose of the Standards

The purpose of the Downtown Urban Design Guidelines is to provide urban design direction and guidance for proposed development at the planning application stage in order to assess, promote and fulfill the intent of the Town's Official Plan policies, Downtown Master Plan vision and Zoning By-law. All work together to influence the overall urban structure and quality of the downtown public realm, with the intent to promote high standards of urban design for both public realm and private realm development and spaces. These guidelines provide a companion document to the implementation of the Official Plan Amendment (OPA 52) and the Zoning By-Law (ZBLA 2018-57) and build upon the design direction and principles that were established during the public consultation process of the Downtown Development Master Plan, March 2017 (DDMP).

1.2 Objectives of the Standards

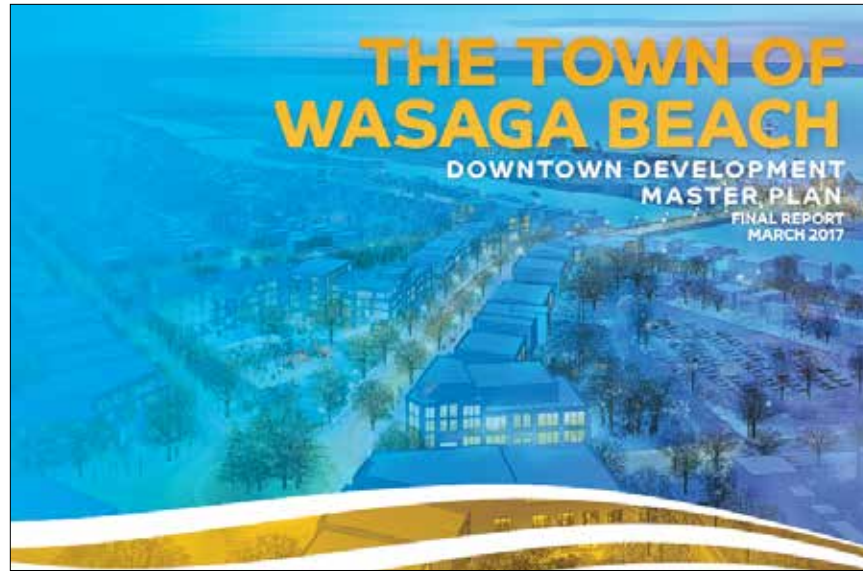
The Urban Design Guidelines, along with the policies contained in Sections 19.20 and 22.3.2 of the Town's Official Plan, set out the urban design policies that support the vision for a vibrant downtown. The guidelines are intended to support the following objectives:

- Encourage development that supports and implements the objectives that are outlined in the DDMP, namely:
 - Build for Multi-Modal Transportation (walking, cycling, transit and the car)
 - Create an Urban Place
 - Establish a Focus for the Community
 - Focus On Programming and Activation
 - Create a Predictable Development Framework
 - Ensure the Plan Is Feasible
- Facilitate a predictable, fair and consistent application of design objectives to development applications;
- Encourage horizontal and vertically integrated mixed-use development within the downtown;
- Encourage high quality built form that contributes to a rich and vibrant public realm;
- Encourage development that is environmentally friendly, safe and universally accessible.

1.3 Expectations

The Downtown Urban Design Guidelines are intended to provide more detailed direction on the Urban Design Policies as set out in the Official Plan. The guidelines outlined in this document are to be addressed and achieved by development proponents through the development application process.

Exceptions to these guidelines as a result of contextual or site specific issues may be considered, at the discretion of the Planning Department and Council, if an applicant can demonstrate that they are still meeting a high level of urban design excellence. The guidelines may be amended on an as needed basis to provide greater clarity as to the vision of the downtown master plan.



2.0 PUBLIC REALM GUIDELINES - DOWNTOWN MASTER PLAN AREA

2.1 General Guidelines

The following list of guidelines provides high level guidance for the major components of the Downtown public realm:

1. Coordinate build-to lines with adjacent properties to create a consistent street wall along frontages.
2. Locate buildings parallel to the street to provide street enclosure.
3. Provide visual interest to front building walls by varying building setbacks.
4. Buildings should incorporate active uses at grade such as retail and other commercial uses to contribute to a more active public realm.
5. Locate main entrances to have direct sidewalk connections and ensure they are flush with the public sidewalk.
6. Locate main entrances so that they are clearly identifiable and visually prominent.
7. Balconies may not protrude into the public realm but may extend as far as the build –to line.
8. Below grade parking structures should have a minimum coverage over slab of 1.5m to facilitate tree planting.
9. Underground parking structures should not protrude into the public realm except in unique situations where parking is intentionally provided underneath public squares.
10. Provide consistent and complementary pedestrian-scaled streetscape design including such elements as decorative and conventional paving, landscaping, lighting, street furniture and signage.
11. Private spaces and activity areas, including building entrances and patios should be oriented toward public roads to act as an interface between private and public realms.
12. Avoid locating building service areas, mechanical equipment and/or ventilation systems into pedestrian areas.
13. Mid-block connections are encouraged to break down block sizes and create a finer grained walking network to encourage pedestrian circulations.
14. The development of a “complete streets” network that accommodates active transportation (walking, cycling and transit) as well as shared street designs are encouraged.
15. Ground floor uses will incorporate transparent windows on the majority of the building facing public areas (e.g. streets, parks, walkways, plazas, and the town square) to promote visually active facades and provide passive surveillance of pedestrian areas.
16. To promote a safe, pedestrian-friendly community, the design of all new buildings should consider the principles of CPTED (Crime Prevention Through Environmental Design).
17. Public art is encouraged throughout the downtown particularly along Main Street and within the proposed Town and Festival Squares. This can include individual works and works that include sequence or common elements that could link one square to the other.

2.2 General Guidelines for Street Trees & Street Furniture

Street trees and boulevard furniture foster the development of a high quality streetscape geared towards the pedestrian scale. They contribute to a livelier main street by providing places to linger, rest and to have chance meetings not only between residents but also those visiting.

1. Street trees should be provided throughout the downtown and be closely spaced at 8.0m to 10.0m on centre and closer where possible or preferred.
2. A tree planting zone of a minimum width of 3.0m should be provided with soil volume for successful establishment and growth.
3. Along Main Street, tree planting zones can be accommodated in planters defined by concrete edging, in raised planters or in tree grates to provide a better urban transition and to widen the pedestrian zone. Where used, tree grates should be easy to remove and maintain.
4. Street trees should be selected that are capable of surviving in Hardiness Zone 5a and should be of native varieties to ensure resiliency. Town Standards for street tree planting should be referred to for permitted varieties ensure resiliency.
5. Street furniture is to be consistent throughout the downtown as follows:

- Bench – Toronto Fabricating & MFG. Co (#1077-S) Colour – Black
- Trash Receptacle - Toronto Fabricating & MFG. Co (#922X2RU) Colour – Black
- Tree Guards - Toronto Fabricating & MFG. Co (#TG-213) Colour – Black
- Tree Grates - Toronto Fabricating & MFG. Co (#B-8650) Colour – Black
- Bicycle Post & Ring - Toronto Fabricating & MFG. Co (#937-SM) Colour – Black
- Bicycle Rack – (Type TBD)



Courtesy of Forrec Limited

2.3 Downtown Core District Streetscape

The Downtown Core District Streetscape refers to the proposed improvements indicated in the DDMP for the portion of Main Street from the Nottawasaga River to Stonebridge Boulevard. This portion of Main Street is within the heart of the downtown and will, in addition to the general public realm guidelines noted above, include the following guidelines:

1. Boulevard widths along Main Street shall be of an adequate width, as noted below, to support comfortable pedestrian circulation, provide area for street furniture and street plantings as well as accommodate spill-over space for adjacent retail/commercial uses including sidewalk cafes and outdoor displays of goods.
2. Streetscape furniture should be of a consistent quality and have complementary design elements to ensure a consistent design language.
3. Where appropriate, on-street parking lay-bys should be incorporated into the boulevard and allow for sidewalk bump-outs at corners to assist in shortening crossing distances at intersections along Main Street.
4. Street lighting along Main Street should be pedestrian-scale and include secondary lighting fixtures for tree planting and pedestrian zones.
5. Pedestrian crossings or mid-block pedestrian connections should be clearly defined and, where possible, incorporate decorative paving materials and/or raised intersections.
6. Service and utility boxes shall not be located within the Main Street boulevards and should be located below grade, within streetlight poles, incorporated into, or located at the rear of Main Street buildings.
7. Public art, at locations along Main Street with adequate space to accommodate it, are encouraged.
8. The incorporation of sustainable infrastructure features such as bioswales, water retention cisterns, permeable paving and planting for stormwater management are encouraged.

9. The improved streetscape right-of-way width in the DDMP has been identified as 30.0m (Figure 1). This road section is comprised of:

- two travel lanes of 3.5m;
- two inset parking layby zones approximately 2.5m;
- a 3.0m multi-use trail defined by green amenity edges on either side of the trail; and,
- a remaining pedestrian boulevard that includes amenity/street furniture zones and pedestrian clearways ranging from 5.5 to 7.0m:

- 3.0m brushed concrete sidewalk
- 2.5m furniture and street tree planting zone with decorative concrete pavers
- Commercial zone (remainder of space) brushed concrete

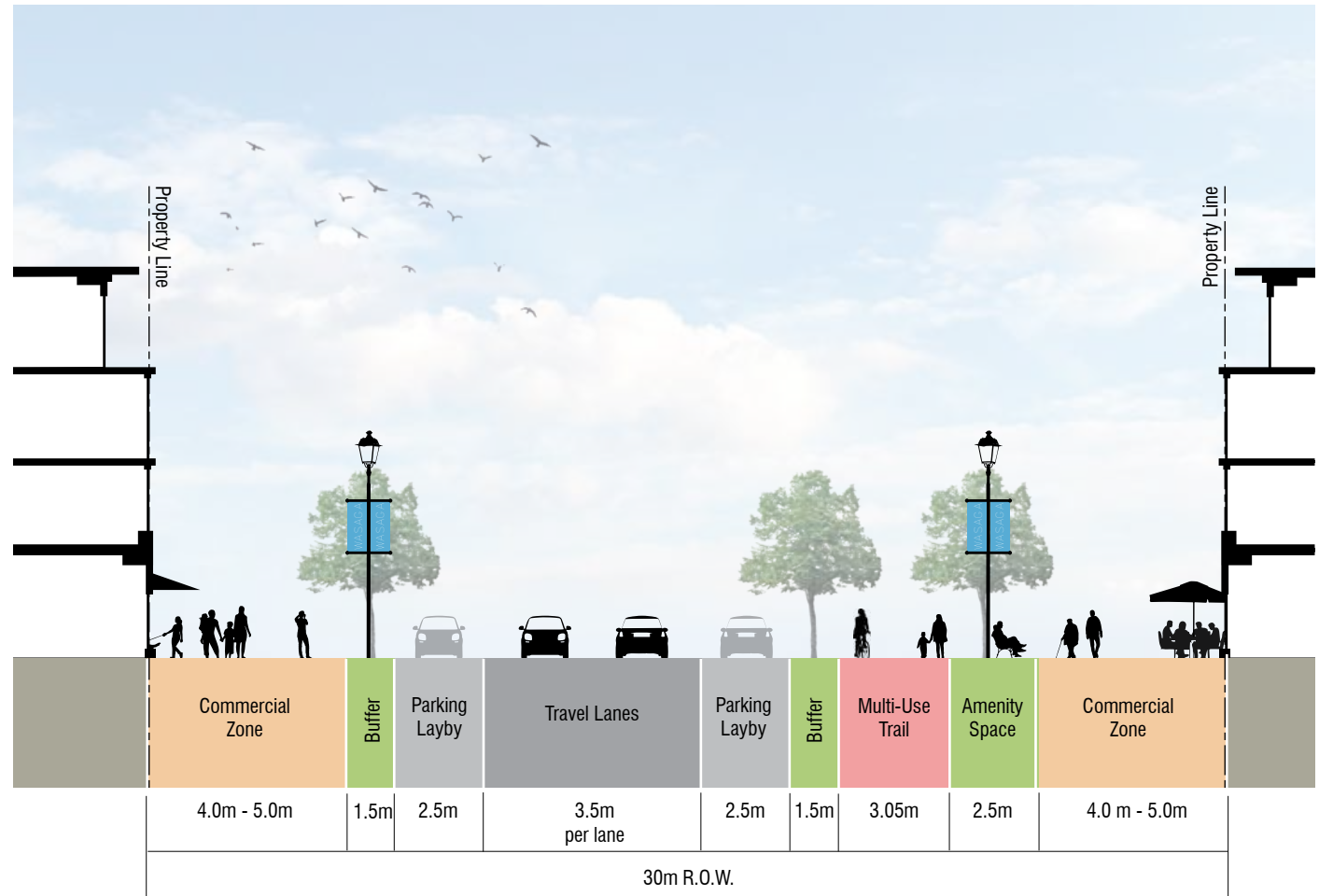


Figure 1 - Main Street Downtown Core & Gateway Districts Street Section (30.0m R.O.W.)

2.4 Town Square

The Town Square is envisioned to be the main focal point in the downtown core for social gathering, lingering and respite, festivals, concerts, markets and significant Wasaga Beach events. In addition to the applicable general public realm guidelines, the following Town Square specific guidelines should be considered:

1. The square will be predominantly hardscaped and include details and demarcations to provide visual interest while defining spaces for social gathering, resting and lingering.
2. Street trees should be provided at the perimeter of the Town Square street edge and should be spaced and arranged to allow the square to function as an event space.
3. The perimeter edges of the town square will be curbsless to allow for the extension of the square onto Main Street and Forest Avenue. These

street portions should be paved to be complementary to the hardscaping in the town square to further define the event space area and to distinguish this area as the focal point in the downtown.

4. At-grade retail/commercial uses fronting onto the square shall include landscape and architectural elements that physically and visually engage the Town Square (e.g. free flowing patio spaces, extensive fenestration, seating areas, weather protection such as canopies, etc.).
5. The Town Square should be designed and programmed to be fully accessible to all during all four seasons.
6. Tree planting within the square should be provided for shade, to define seating areas (permanent and seasonal) and to distinguish programme spaces within the square.

7. Public art should be provided within the square to provide further emphasize its significance as the main public gathering space in the downtown core. Public art is encouraged to incorporate interactive elements and seating areas, where appropriate.
8. Lighting within the square should be complementary to the hard

and soft landscaping in the square and their layout/type shall be integral to the landscape plan for the square.

9. Utility and transformer boxes shall not be permitted in the square. They shall be located below grade, within streetlight poles, within built form or at the rear of building facing onto the square.





Courtesy of Forrec Limited

2.5 The Beach District Streetscape, Festival Square & Beachfront

The Beachfront District is envisioned to be a year-round destination catering to residents and tourists. It will prioritize pedestrian circulation and include a new gathering area (proposed Festival Square) and improved streetscape and beachfront relationships. In addition to the general public realm guidelines the following area specific guidelines should be considered for the Beachfront District:



2.5.1 Beach Drive / Boardwalk

1. The design of the boardwalk should be raised to help mitigate against wind erosion of beach sand while providing for views of the bay that is accessible for people of all ages and abilities.
2. The Beach Drive should be curbless to act as a continuous pedestrian promenade when the area is closed to vehicular traffic.
3. The proposed improved streetscape for the Beach Drive in the DDMP indicates a right-of-way width 30.0m (Figure 2). This road section is comprised of:
 - two vehicular travel lanes of 3.5m;
 - a 3.0m amenity space zone (combination of concrete unit pavers and softscape);
 - a 5.0m brushed concrete pedestrian clearway along the built form edge;
 - a zone for a raised terrace at the built form edge (width TBD) with decorative pavers; and,

- a generous boardwalk zone that includes pedestrian and cycling use while providing opportunities to rest and take in views of the waterfront and streetscape including a:
 - 3.0m asphalt cycling path

- 3.0m boardwalk (surface TBD)
- 2.0m softscape separation zone between cycling and boardwalk

4. Streetscape furniture and lighting shall not obstruct the travel portion of cycling and pedestrian zones along the boardwalk beach frontage. It should engage both the beach and street edges.

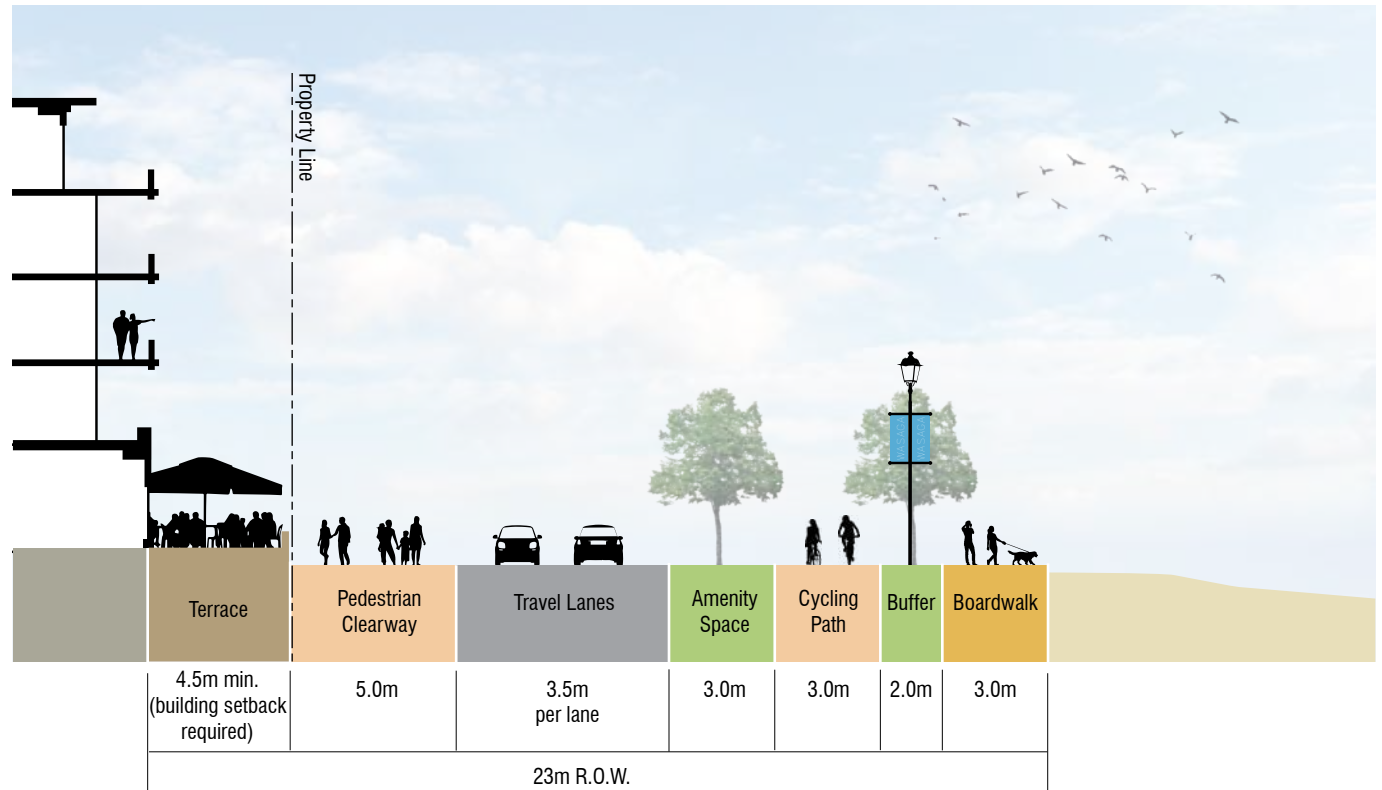


Figure 2 - Beach Drive / Boardwalk Street Section (23.0m R.O.W.)

2.5.2 Festival Square

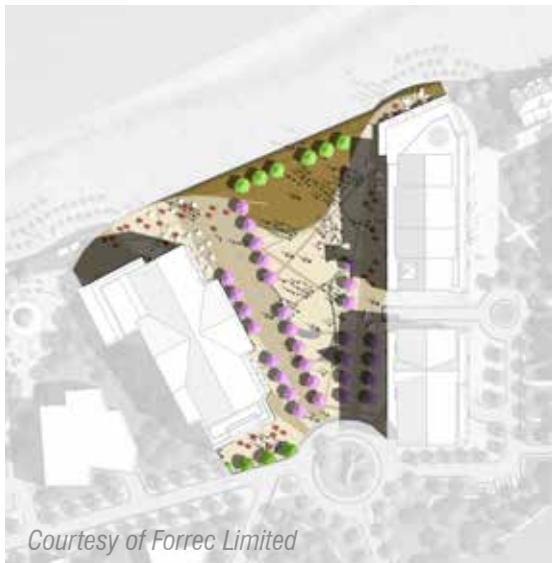
Festival Square will serve as a new focal point within the beachfront area and will serve as destination point for pedestrians approaching from Main Street and from the boardwalk and will provide views to the beach and lake.

1. The square will be predominantly hardscaped and include details and continuous paving design from one building frontage to the opposite building frontage.
2. The paving design should provide for visual interest in the square, demarcate spaces for vendors

and outdoor events and be complimentary to the built form colours/materials in the square. It should accommodate limited vehicular access for servicing and delivery access.

3. Ample tree-shaded seating areas are encouraged throughout the square. Trees could be used to define pedestrian and vehicular travel zones within the square and programmed spaces.
4. At-grade retail/commercial uses fronting onto the square shall include landscape and architectural elements that physically and visually engage Festival Square (e.g. temporary seating furniture and umbrellas, patio spaces, extensive fenestration, weather protection features, etc.).
5. The square shall be designed and programmed to be accessible to all and for use during all four seasons.
6. Public art should be provided within the square to emphasize a programme element within the square, to address views to the waterfront or to provide a visual anchor for social gathering.

7. Lighting within the square should be complementary to the hard and soft landscaping in the square and their layout/type shall be integral to the landscape plan for the square.
8. Utility and transformer boxes shall not be permitted in the square. They shall be located below grade, within streetlight poles, built form or at the rear of buildings and screened from view.



2.5.3 Mosley Street (Beach District)

The portion of Mosley Street will serve a more functional purpose within the area by providing a connection to the resort and residential uses envisioned in the Beach District. It will differ from Beach Drive as it will provide a balance of access for private vehicles and active modes of transportation. It will allow for vehicular circulation and access to parking areas in the Beach District when Beach Drive is closed to vehicles during special events. In addition to the general public realm guidelines the following area specific guidelines should be considered:

1. The design of the street will include a more conventional street section that will address pedestrian and vehicular circulation dedicated to accessing residential and resort functions not immediately adjacent to the beachfront.
2. The proposed improved streetscape for Mosley Street will have a right-of-way width

of 23.0m (Figure 3). This road section is comprised of:

- two vehicular travel lanes of 3.5m;
- a 3.0m street tree and

street furniture zone with decorative unit pavers on each side of the road;

- a 3.0m brushed concrete pedestrian sidewalk;

- a 3.05m multi-use trail with a 1.5 m buffer zone defined by decorative pavers;

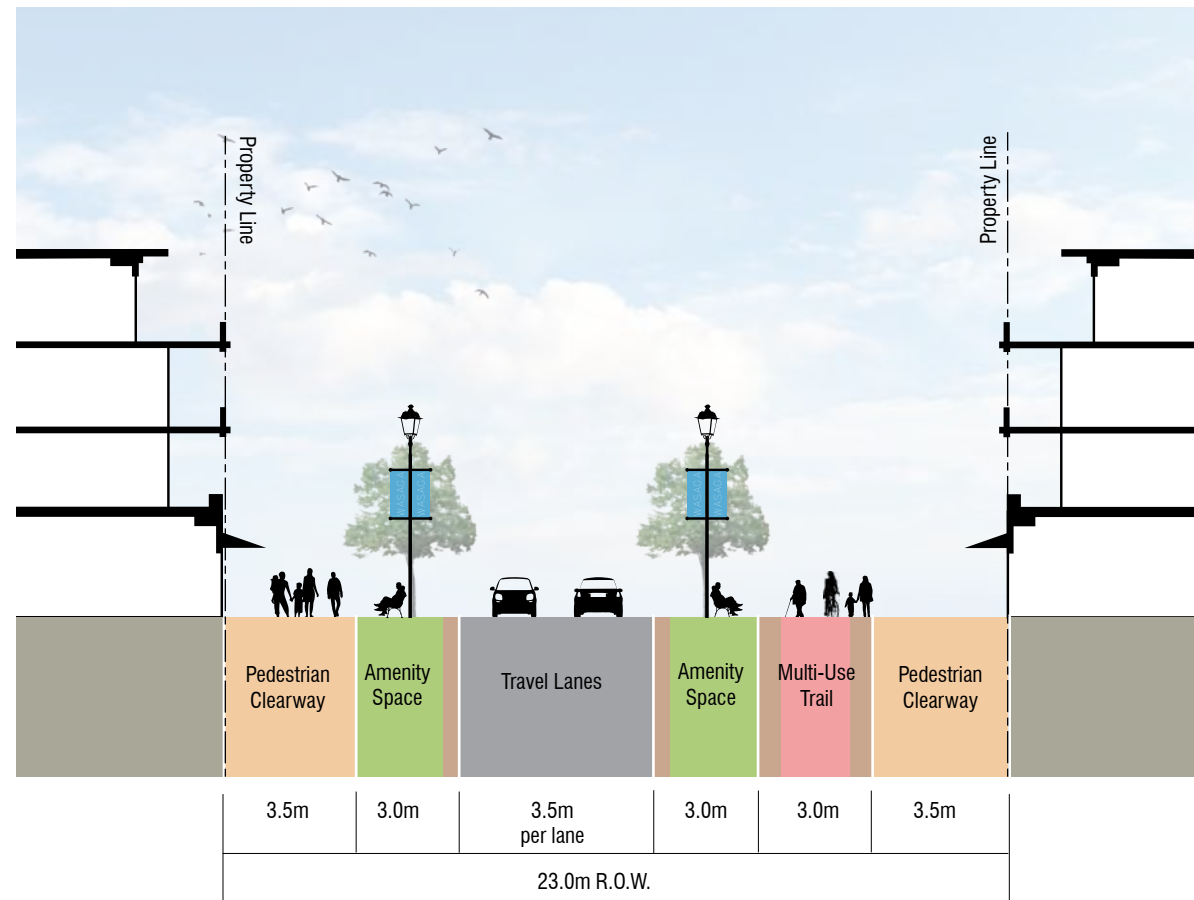


Figure 3 - Mosley Street Section (23.0 m R.O.W.)

2.6 Downtown Gateway District Streetscape

The Downtown Gateway District Streetscape refers to the portion of Main Street from Stonebridge Boulevard to River Road West. In addition to the general public realm guidelines noted in 2.1, the following guidelines apply:

1. Sidewalk widths along Main Street shall be a minimum of 2.4m - 3.0m to accommodate related street furniture and support comfortable pedestrian circulation.
2. Streetscape furniture should be of a consistent quality and have complementary design elements to ensure a consistent design language.
3. Where appropriate, on-street parking lay-bys should be incorporated into the boulevard and allow for sidewalk bump-outs at corners to assist in shortening crossing distances at intersections along Main Street.
4. Driveway access from Main Street is discouraged and where present should be shared between adjacent properties. A contiguous and connected parking area will provide better vehicle circulation and minimize the number of interruptions to the pedestrian boulevard.
5. Where driveways from Main Street provide access to parking areas, driveway widths shall be kept to a minimum (6.0m maximum or minimum width permitted in the zoning by-law) to limit streetscape and pedestrian path interruption.
6. Driveway curb radii should be reduced to increase areas for planting and to help compress the pedestrian crossing of the driveway leading to the parking areas.
7. Driveway access from Main Street for drive-through retail shall not be permitted, in keeping with the private realm built form requirements for Main Street.
8. Where surface parking or service areas are exposed to public view, their visual impact shall be mitigated with landscaping and/or other design measures that include as a minimum:
 - a 3.0m deep landscaped edge along the street in line with its adjacent building located at the front property line including fencing or planting; and,
 - a 3.0m planted side yard setback between the parking area and the property line.
9. Street lighting should be scaled to the Main Street streetscape style and include secondary lighting fixtures for tree planting and pedestrian zones.
10. Pedestrian crossings or mid-block pedestrian connections should be clearly defined, signed and, where possible, incorporate decorative paving materials.
11. Service and utility boxes shall not be located within the Main Street boulevards and should be located below grade, within streetlight poles, incorporated into, or located at the rear of Main Street buildings and screened from view.
12. The incorporation of sustainable infrastructure features such as bioswales, water retention cisterns, permeable paving and planting for stormwater management are encouraged, where feasible.
13. The improved streetscape right-of-way width in the DDMP has been identified as 30.0m for the Downtown Core District shall generally apply to the Downtown Gateway District (Figure 1). This road section is comprised of:
 - two travel lanes of 3.5m;
 - two inset parking layby zones approximately 2.5m;
 - a 3.0m multi-use trail defined by green amenity edges on either side of the trail; and,
 - a remaining pedestrian boulevard that includes amenity/street furniture zones and pedestrian clearways ranging from 5.5 to 7.0m:
 - 3.0m brushed concrete sidewalk
 - 2.5m furniture and street tree planting zone with decorative concrete pavers
 - Commercial shy zone (remainder of space) brushed concrete

2.7 Downtown Gateway & Roundabouts

The Downtown Gateway area is defined by a significant roundabout that provides a visual cue of arrival at a major entry into the downtown from River Road West. Gateways are defined by a combination of landscaping and built form. The following guidelines apply to the public realm:

1. Landscaping proposed at gateways should complement and visually coordinated with signage, built form and potential public art proposed for the gateway (see section 3.10 for private realm guidelines) at Main Street and River Road West.
2. Gateway landscaping shall be of a high quality design in soft and/or hard landscaping comprised of resilient and durable materials and planting.
3. Landscaping proposed for roundabouts shall be planted with low maintenance native plant species and should not impair safe sight lines, with taller plantings located at the centre and shorter plantings on the outside.
4. The minimum radius for roundabouts shall be subject to Town approval and designed in accordance with Provincial guidelines and the standard road widths proposed in the downtown.
5. Public art pieces should be considered for the roundabout to signify and further emphasize the sense of arrival.
6. The gateway and roundabout area should include consistent paving materials, colours, details and textures to create a stronger visual presence and emphasize the sense of entry to the downtown.
7. Nighttime presence of the gateway and roundabout design should be considered in terms of visibility, change of seasons through lighting and landscape features that accentuate this entrance.
8. Utility locates adjacent to a gateway feature shall be incorporated into the design of the gateway feature, located below grade or be screened from public view.
9. Gateways could include differentiated streetscape features or details such as Wasaga Beach signage, street arches, columns, water features or other landscape feature as may reflect the vision for the Wasaga Beach Downtown.



Courtesy of Forrec Limited

Figure 5 - Downtown Gateway Concept Plan

2.8 Hierarchy of Downtown Streets

A hierarchy of streets has been defined for Downtown Wasaga Beach and noted in the figure to the right as 'A' and 'B' Streets. The 'A' Streets represent key streets that will promote pedestrian and retail/commercial activity as well as other mixed-uses within the downtown and include Main Street, Beach Drive, and the portion of Mosley Street within the Beach District. The streetwall along 'A' Streets should be uninterrupted and not provide access to parking, loading or servicing areas which should be directed to adjacent 'B' Streets.

The 'B' Streets represent existing or proposed future streets that contribute to connectivity of the downtown while directing activity to the 'A' Streets and their servicing, loading and parking areas. The 'B' Streets will range from predominantly residential uses in the north-south streets indicated but will also include streets with a range of uses such as River Road East (Main to Beck Street), Beck Street, River Road

West (Beck to Main Street Gateway) and those existing, or proposed, in the Beach District to accommodate the tourist and hospitality functions envisioned for the area. The 'B' Streets should adhere to the general public realm guidelines

however they may differ in the following ways:

- the sidewalk can range in width from 1.8m to 3.0m depending on the context and location of the street; and,

- the requirement for street trees will be evaluated on a case by case basis for each development application.



Figure 6 - Downtown Street Hierarchy

3.0 PRIVATE REALM GUIDELINES - DOWNTOWN MASTER PLAN AREA

The purpose of the guidelines is to provide direction on appropriate built form with respect to their location within the Wasaga Beach downtown. The built form guideline framework outlined in this section will provide the built form complement to the public realm boulevard to create a vibrant and pedestrian scaled environment in the Downtown Master Plan Area.

The general guidelines that follow provide standard requirements for all private realm built form in the downtown. The guidelines following

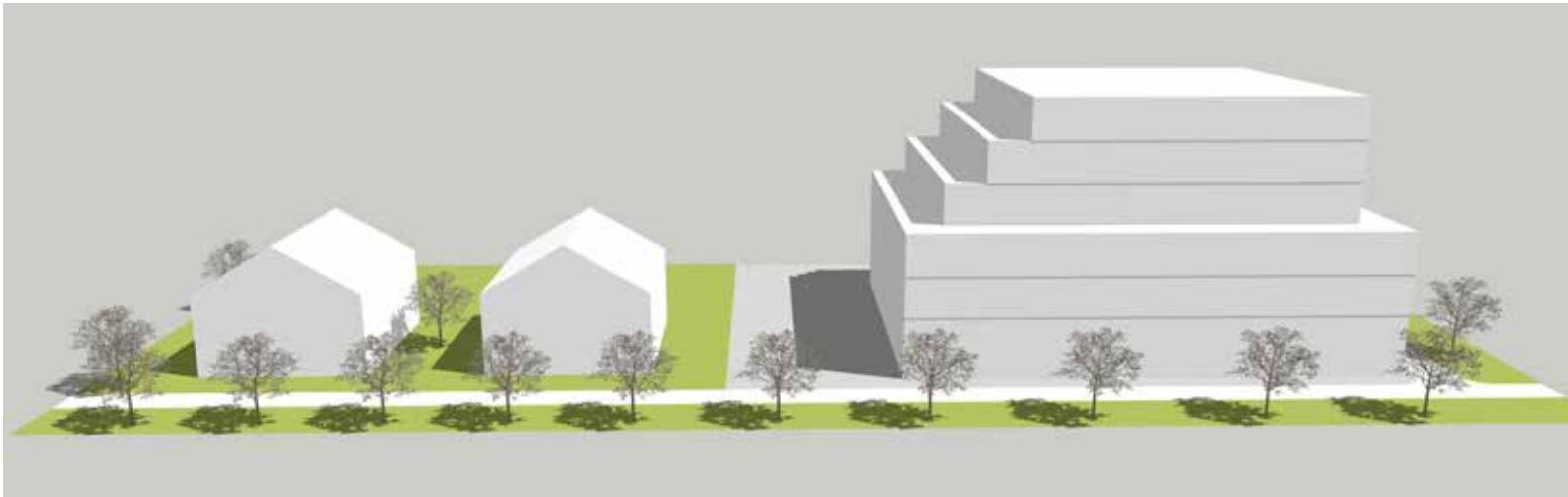
this section provide more guidance with respect to specific built form typology (sections 3.2 to 3.5) and to special areas identified in the DDMP (sections 3.6 to 3.10).

During the development application process, a development concept plan, building elevations, and landscape plan will be required for development in the downtown area. They shall be accompanied with a memorandum or urban design brief, indicating how the urban design guidelines have been addressed in the proposed development.

3.1. General Built Form Guidelines & Building Siting

3.1.1 Massing, Height and Setback

1. Buildings shall be oriented to address the street and provide clearly defined entrances that directly connect to the sidewalk.
2. Building massing should reinforce a continuous street wall frontage, located at the front property line to help define the pedestrian boulevard. Building frontage can be set back up to 3.0m in certain areas to accommodate building entrances or patio areas.
3. Main Street proposed streetscape right-of-way improvements will require that building face to building face distance will be approximately 30.0m.
4. A variety of architectural elements such as wall plane articulation, defined entries, canopies, columns, dormers, material detailing will be employed to create a distinctive character for the Main Street streetscape.
5. More prominent building massing and articulation should be provided at corners and especially at the gateway entrances to the community. This added treatment highlights the significance of these intersections and may frame views and vistas to the waterfront.
6. Buildings on corner lots shall be oriented to address both streets and generally located close to the street edge. Where corner lot rear elevations are exposed to street view they shall be consistent in architectural design and quality with the front and external side elevations.
7. Building heights of new developments shall range from 2 storeys for low-rise residential to 6 storeys for mixed-use residential buildings on Main Street.

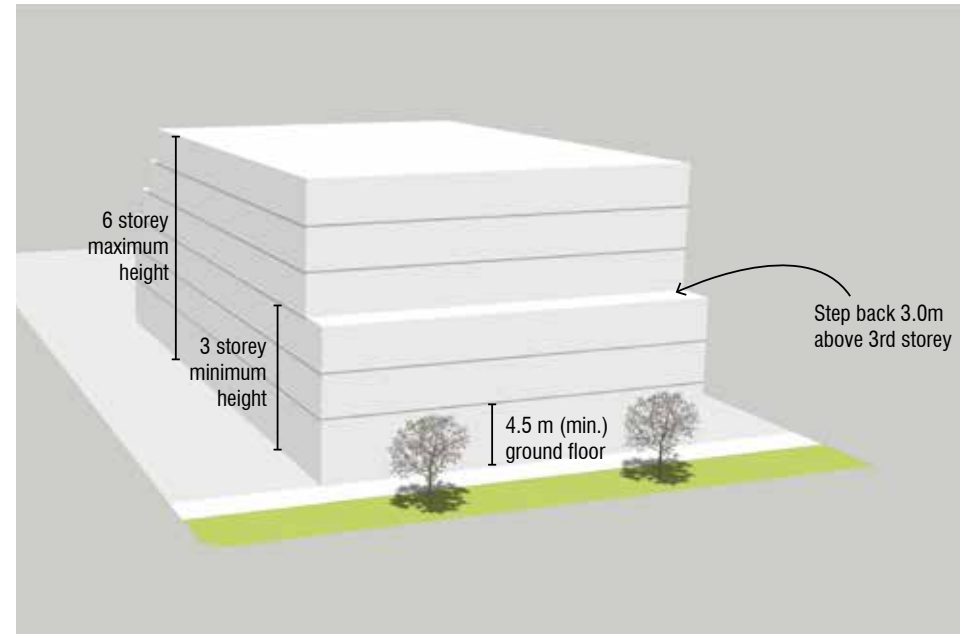


Example of built form massing relationship

8. Where building heights exceed 3 storeys, a step back of 3.0m shall be provided beginning with the 4th storey along a 45-degree angular plane measured from the opposite streetline. This will be applied along the building frontages on Main Street, Beach Drive, Mosley Street, abutting the two public squares in the downtown, and any other streets that permit over three storeys, in order to minimize sun shadow impacts.
9. Mixed-use building with retail/commercial/office uses at grade shall have a minimum ground floor height of 4.5m to provide building flexibility and a pedestrian-scaled edge.
10. Building frontages are key in forming the continuous pedestrian friendly street edge along Main Street in the

Downtown Core District area and at the Town and Festival Squares. Allowances may be made to allow for mid-block pedestrian connections.

11. Blank wall faces are discouraged especially along the rear elevations of the Main Street and Beach District mixed use and hotel buildings. The building shall be clad and detailed consistently on all exposed elevations.
12. Rooftop mechanical equipment shall be screened from public view through roof-top location, integration into the design of the building and/or enclosures clad in materials and/or colours that are consistent or complementary to the building.
13. Building designs are encouraged to be solar ready and constructed to allow for future solar installations and/or other sustainable design features.



General height and massing for main street buildings

3.1.2 Private Parking & Servicing for Main Street Properties and Squares

1. Parking areas and servicing shall be located in the rear of the development or, preferably, below grade.
2. Large parking areas should be broken up into smaller courts of parking divided by landscaping and include planted medians, a minimum 2.5m in width with 3.0m preferred, to accommodate tree planting.
3. Defined pedestrian walkways through raised curbs or differentiated paving should be provided to provide a safe connection between rear parking areas and the rear entries of buildings.
4. Within large parking areas a pedestrian walkway spine within parking lots/courts should be provided. It should be a minimum 4.5m wide to accommodate a 2.4m walkway and a 2.5m continuous planted edge on one side for tree planting and landscaping.
5. Wherever possible the use of permeable or porous pavers is encouraged along with extensive soft landscaping to minimize stormwater and integrate into stormwater management for the site.
6. The use of native tree and shrub species should be prioritized and be low maintenance, salt tolerant and able to survive urban stress conditions within parking areas.
7. Crime Prevention Through Environmental Design (CPTED) principles shall guide landscape design and landscaping should not obstruct sight lines for vehicles or pedestrians.
8. Designated parking (e.g. accessible parking spaces, bicycles, and electric or energy efficient vehicles) should be located close to building main entrances.
9. Snow storage areas should be identified and incorporated into the overall landscape plan for surface parking areas.
10. Environmentally friendly features within parking courts such as solar canopies or electric vehicle charging canopies are encouraged.
11. Locate service areas including loading and garbage, in low visibility locations at the rear and screen with wing walls where highly exposed.
12. Architectural screening or enclosures of service areas should be built with materials/colours complementary to the building style.
13. Transformers and other utilities should be located within buildings, below-grade, screened from public view, within streetlight poles, or located inconspicuously at the rear of the property.



3.2 Infill Detached and Semi-Detached Dwellings within the Downtown

New infill detached or semi-detached residential units will be primarily located in areas of the downtown that are adjacent to existing low-rise ground related residential uses. They provide for sensitive massing transition to the mixed use built form envisioned for the downtown and in particular Main Street.

1. Buildings shall be placed parallel to the road with front doors, windows and entry features facing the road to create a consistent built form street edge.
2. Where front loaded garages are present, the setback to the main building face should be from 4.5m to 6.0m from the edge of the right-of-way. The setback will be measured to the main building face or front entrance.
3. Garages shall be set behind the main building face or porch or be accessed from a rear lane. Dwelling designs with garages projecting beyond the front building wall face shall not be permitted.

4. Dwelling designs will consist of single garage doors and driveways to limit the physical and visual impact of garages on the streetscape and pedestrian use. Where possible, access to a garage at the rear of the lot, accessed by a lane is preferred.
5. Dwellings with front loaded garages shall pair driveways with adjacent houses to maximize a continuous green planting area and reduce the amount of asphalt on front yards.



6. Corner lot dwellings and homes facing or abutting parks / open spaces are highly visible and important streetscape anchors. The design of these homes shall include the following:
 - The flanking side elevation and rear elevation shall be given a similar level of architectural detailing (windows, materials, and details) equal to the front elevation of the house;
 - The main front entrance should be located on the exterior side elevation, where possible and corner windows, wrap-around porches, or other architectural feature should be included to address the corner location; and,
 - The rear elevations of both semi-detached units on a corner lot shall be upgraded to be consistent with the front and side elevations of the building.



3.3 Townhouses & Stacked Townhouses

7. Porches, stairs, canopies and other entrance features may encroach into the required setbacks.
8. Single and semi-detached dwellings should have two to three storey massing.
9. For semi-detached corner lot buildings, the entry feature of the interior unit should be oriented to the front lot line, while the entry of the corner unit is encouraged to be oriented to the flanking lot line.
10. Projections into the front yard, such as porches, entrance canopies, porticos, entrance steps and bay windows are encouraged for ground-related dwellings to provide pedestrian-scaled streetscape interest.

Townhouse dwellings are comprised of individual units attached and grouped together into a larger architectural form. This type of built form is appropriate in transition areas adjacent to the Main Street. In addition to the *Town of Wasaga Beach Townhouse and Apartment Built Form Urban Design Guidelines (2017)* requirements, new infill townhouse design will also consider the following guidelines:

1. The siting, massing, and façade design of townhouse units shall be coordinated on a block-basis. The overall streetscape composition should display massing and design continuity with adjacent development while achieving streetscape variety.
2. The elevations of the townhouse block shall be articulated in a manner that provides variation between units, but reinforces common characteristics that visually unite the townhouse block.



3. Variety in the design of roofs through the use of traditional gables and dormers, or more contemporary designs that include cantilevers and parapet details is required to break up the massing of townhouse blocks. However, the main roof should appear as one roof where possible and reflect the architectural style of the townhouse block.
4. The massing and form of townhouse units adjacent to single/semi-detached dwellings shall be complementary to those dwellings through height and architectural features to promote transition and visual continuity along the streetscape.
5. The main front entry shall be oriented to the front lot line or higher order street. While the entry of a corner unit is encouraged to be oriented to the flanking lot line, where a unit flanks a laneway, the main entrance shall face the public street.
6. Townhouse units with rear lane accessed garages are preferred, while townhouse units with front loaded garages are discouraged in the downtown.
7. Detached lane accessed garages shall be complementary in design and building material with the principal dwelling.
8. Utility meters shall be screened from public view by integrating them into the design of the townhouse units through the use of wall recesses, enclosures, or inseting within the building walls. Rear lane townhouses should, where possible, locate utility meters at the rear of the lot.
9. Side and rear elevations highly visible from public areas shall have architectural treatments consistent with the design of the front elevation.
10. Corner unit designs are encouraged to provide significant window openings, wall articulation and porch features appropriate to their architectural type and style.
11. Townhouse blocks will be limited to a maximum of 8 units and will be considered on a site by site basis. The maximum length of townhouse blocks should not exceed 48m; greater block lengths will be considered based on architectural merit.



3.4 Low Rise Apartment Buildings

Low-rise apartment buildings are appropriate in establishing an active urban character where intensity of use is desirable, such as locations adjacent or close to, but not on the Main Street. They should generally range in height from 4 (14.0m) to 6 storeys (20.0m).

In addition to the Town of Wasaga Beach Townhouse and Apartment Built Form Urban Design Guidelines (2017) requirements, new apartment design will also consider the following guidelines:

1. The majority of the main building facade shall be oriented to front the public road within the minimum setback.
2. The primary building entrance shall be located and oriented toward the street frontage, provide weather protection and visibility to interior lobbies, to allow for safe and convenient arrival and departure from the building.
3. Permanent parking shall be located below grade where feasible. Loading and service

areas shall be located in areas of low public visibility in side or rear yards. These functions shall not occur along the lot frontage.

4. Where it is only possible to provide parking at grade, it shall be located at the rear of the lot and be screened from street view by the building.
5. The design of the building and the site layout shall consider overall form, massing and proportions, and rhythm of major repetitive building elements, to create a street façade that supports the pedestrian scale.
6. A variety of high quality masonry (brick and stone) or other quality building cladding systems will be considered. Concrete block, precast wall panels or metal siding are discouraged.
7. Where a building exceeds 3 storeys, a stepback of 3.0m should be provided at each storey above the 4th storey along a 45-degree angular plane measured from the opposite streetline to provide a pedestrian-scaled streetscape and minimize sun shadow impacts.

8. Rooftop mechanical equipment shall be screened from public view and integrated into the design of the building with materials and/or colours that are complementary to the building.



3.5 Guidelines for Mixed-Use Buildings

Mixed-use buildings shall be the predominant built form along Main Street and abutting the proposed Town and Festival Squares. This built form type can include commercial and office uses at grade and multi-unit residential uses above.

3.5.1 General Guidelines for Mixed-Use Buildings

1. The guidelines outlined for Low-Rise Apartment Buildings, shall also apply to mixed-use buildings.
2. They will range in height from a minimum of 3 storeys (11.0m) to 6 storeys (20.0m).
3. Where a building exceeds 3 storeys, a stepback of 3.0m should be provided. The stepback should start at the 4th storey along a 45-degree angular plane measured from the opposite streetline. This will provide a pedestrian-scaled streetscape, which minimizes shadow impacts, and provides a continuous roof line along main streets at the cornice of the 3rd storey.
4. The design of mixed-use buildings should reflect the mix of uses occurring with the building and should be used to establish a strong pedestrian oriented grade level commercial / retail uses.
5. Larger buildings should be articulated to avoid large expansions of uninterrupted blank facades and grade level retail frontages should be broken down in scale to provide a finer grained frontage onto focal areas such as Main Street and the Town and Festival Squares.
6. Residential entrances shall be clearly distinguished from the commercial entrances and can be located at the front or side of the building.
7. Where located at a corner, buildings shall be designed to address both street frontages and be massed towards the corner locations for visual interest along the streetscape and to visually anchor the building.
8. Large ground floor retail windows shall be provided and be appropriate in scale to the street frontage and integrated into the building design. Reflective mirror glass shall not be used for



windows at grade.



3.5.2 At-Grade Retail Design

1. For the ground floor facade, a minimum of 80%-90% of the area between 0.6m and 3.0m from the finished floor, shall be clear glazing with added clear glazing beyond that is encouraged.
2. Glazing – no tinting is permitted. Must be able to see at least 0.9m past the glazing. Three dimensional displays may be within 0.9m of glazing. Posters, boards, signs, decals and other flat or near-flat objects or visual obstacles cannot cover more than 20% of the glazed area.
3. A minimum floor to ceiling height of 4.5 metres should be provided on the ground floor to allow for flexibility and accommodate a variety of retail/commercial/office uses.
4. Clear glazing should be used for all wall openings (e.g., windows and doors) along the street-level façade. Dark tinted, reflective or opaque glazing shall not be permitted for storefronts.
5. On corner sites, storefronts should address both street frontages through entries and/or clear glazing.
6. Storefront entrances should be highly visible and clearly articulated. Entrances should be located at or near grade. Split level, raised or sunken entrances shall not be permitted.
7. Storefronts and buildings should be designed to articulate a rhythm of different buildings at intervals of every 6 to 10m wide, created through the subdivision of retail space, the use of changing building materials and/or façade articulation.
8. Retail space should have a minimum unit depth of 10m to ensure reasonable operation.
9. Retail unit widths within the Downtown Core District will have a maximum width of 10m.
10. For buildings that are open to the public, the front doors must be active, unlocked and usable for pedestrian access to and from the building by the public during normal business hours.
11. Door function should not employ space beyond the face of the building in order to maximize the use of the sidewalk and pedestrian space.
12. Individual retail windows should be perceptibly taller than they are wide.
13. Finished floor elevation within 5 to 15cm of the sidewalk elevation

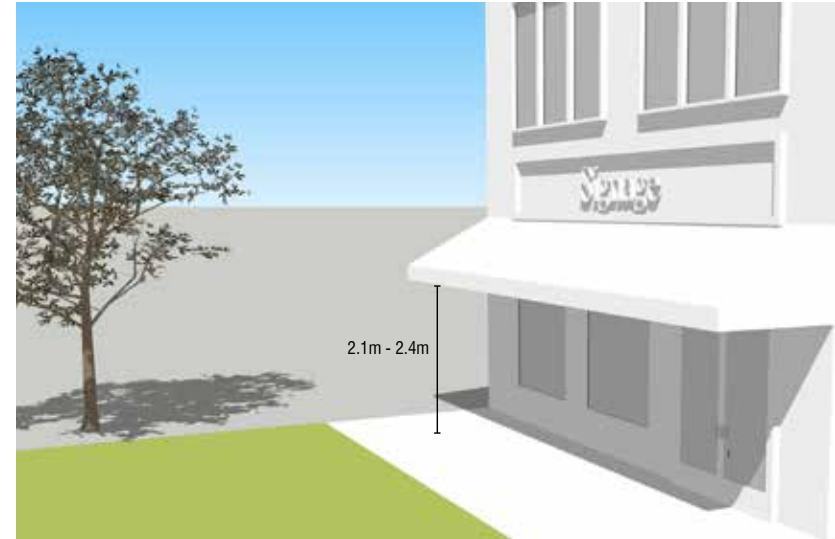
on the right-of-way line at the front access, and should provide a ramp for full accessibility.

3.5.3 Awnings and Canopies

1. Continuous awnings or canopies are encouraged along 100% of building frontages along sidewalks and walkways to protect pedestrians from seasonal elements.
2. The use of awnings and canopies provide for an added element of articulation on the building elevation and contributes to a pedestrian scaled streetscape edge.
3. Materials used for awnings and canopies should be carefully selected to be of high quality

material. Materials that are strongly discouraged include aluminum, vinyl or plastic.

4. The bottom of the awning should be between 2.1m and 2.4m above the sidewalk or at a height that does not obstruct store front entrances.
5. Awnings should protrude into the right-of-way at a depth that provides weather protection but does not conflict with street trees and furniture (e.g. 2.0m to 2.5m depth).
6. The colour of awnings/canopies should be coordinated with the colour package of the buildings main cladding materials and color scheme.



3.5.4 Signage

1. The location of signs should be placed consistently across all building façades. Generally, signs should be located above the storefront windows and within an articulated sign band, or on canopies over the storefront.
2. Signage should not present as an obstruction to windows, cornices, columns or other architectural elements. Signage area should not exceed an area of 20% of the ground floor retail frontage.
3. Signage materials should be durable, weather resistant, and complementary to the materials of the building façade.
4. Signs that use lettering and/or images that create depth to the sign, such as raised lettering or individually cut lettering and are lit from above or below are encouraged. Box signage with internal lighting shall not be permitted.
5. Projecting/hanging signs, perpendicular to the building façade will be permitted, provided that they do not project more than 1.0m from the building, have a minimum 3.0m clearance between the bottom of the sign and grade and be limited to a surface area of 1.0m squared.
6. All signage shall conform to the Town of Wasaga Beach's by-law and regulations.



3.6 Downtown Core District Built Form Guidelines

The Downtown Core District is located at the heart of the downtown and is identified as one of the main focus areas for implementation in the DDMP. The vision for the Downtown Core District is to create a compact mixed-use form that caters to a range of uses including employment, retail and residential units.

3.6.1 General Guidelines for the Downtown Core District:

In addition to the general guidelines outlined in 3.5 Guidelines for Mixed-Use Buildings, the following shall be considered for buildings in the Downtown Core District:

1. Buildings up to 6 storeys shall be permitted within the Downtown Core District area, with a minimum height requirement of 3 storeys. Added height may be considered on a case by case basis and considered on architectural merit and context.
2. The ground floor uses in the Downtown Core District should be primarily retail and commercial uses except for entrances to units above. Uses above the ground floor shall include residential and office uses.
3. Retail uses should include cafes, restaurants, coffee shops, bars/pubs, neighbourhood services. Drive-through retail uses shall not be permitted along the extent of Main Street.
4. Building façades in the Downtown Core District should extend for 100% of the lot frontage. Exceptions will be permitted where mid-block connections or walkways are introduced or has architectural merit.
5. Interruptions to the streetscape for vehicular access driveways, servicing or loading areas shall not be permitted.



3.6.2 Town Square Anchor Built Form

Located at the centre of the Downtown Core District area, the Town Square and its surrounding buildings will serve as the anchor for the district and the heart of the downtown core. The buildings will include a mix of uses which in addition to commercial and residential uses, may include municipal or other civic functions to reinforce the Town Square anchor. In addition to the general guidelines outlined in 3.5 Guidelines for Mixed-Use Buildings, the following shall be considered:

1. Built form fronting on the town square shall frame and define the inner edge of the square.
2. Buildings at the edges of the square, flanking Main Street and Beck Street, shall address both frontages with equal architectural detail.
3. The siting of municipal and civic uses such as municipal offices, a library, theatre and arts facilities and cultural program spaces are encouraged.

4. Built form shall have a minimum height of 4 storeys and a maximum height of 6 storeys. The added height in comparison to Main Street is meant to emphasize visually the importance of the Square.
5. A 3.0m stepback will be provided beginning at the 5th storey and will be applied for each additional storey above.
6. Buildings flanking Main Street or Beck Street should include added architectural emphasis such as tower elements or other similar landmark feature to visually indicate the location of the Square. Such features may exceed the maximum height.
7. Ground floors of these buildings will be reserved for active commercial / retail uses and restaurants to ensure that there are active uses adjacent to the Square. Such uses should function throughout the day and especially the evening to engage the Square and its visitors.

8. The ground floor uses should exhibit a fine grain and scale of restaurant / retail / commercial to ensure variety and the potential for continuous activity. Large scale retail shall be discouraged to avoid the predominance of singular use that may not activate

the frontage onto the Square throughout the day.

9. Town Square building massing will transition to adjacent building through a combination of stepping of storeys or architectural detailing.



3.6.3 Community Hub Buildings

The proposed Community Hub, located in the vicinity of the southeast corner of Main Street and Beck Street is envisioned to host indoor events and activities that cater to a range of activities year round. This could potentially include sports and cultural events, library and museum uses, and performance spaces for music and art. In addition to the general guidelines outlined in 3.5 Guidelines for Mixed-Use Buildings and 3.6 Downtown Core District Built Form Guidelines, the following shall be considered:

1. Built form shall have a minimum height of 3 storeys and a maximum height of 6 storeys.
2. A 3.0m stepback will be provided beginning at the 4th storey and will be applied for each additional storey above.
3. The corner of the Community Hub, at Main Street and Beck Street, shall address both frontages with equal architectural detail.

4. For the public space / plaza envisioned for the Hub, built form should address this space through a variety of at grade uses that cater to both the functions of the building and Main Street.
5. Built form should provide for added weather protection at the edges of the public space to ensure comfort and use throughout the year.
6. The paving materials for the public space should complement the Community Hub cladding materials and may reference the Town Square paving materials for visual continuity with respect to its civic nature.



3.7 The Beach District Built Form Guidelines

Located along the waterfront of Wasaga Beach, the Beach District is envisioned as an entertainment activity centre that offers diverse activities for families including indoor and outdoor entertainment year round. The introduction of Festival Square as a focal point in the district, an entertainment zone and added variety of accommodation will differentiate this area within the downtown and broaden its appeal as a destination. In addition to the general guidelines outlined in 3.5 Guidelines for Mixed-Use Buildings, the following shall be considered:

3.7.1 General Guidelines for the Beach District Area:

1. New buildings proposed within the Beach District should range in height between 3 and 6 storeys in height.
2. Buildings, wherever possible, shall be designed to frame views to Georgian Bay and to address features such as the waterfront, street frontages and Festival Square.
3. The scale of at-grade retail will range from small to large scale retail / commercial uses to accommodate tourism requirements and reinforce the district as a destination.
4. The ground floor height shall be a minimum height of 4.5m. Where increased floor heights are presented they shall be considered on their proposed function and architectural merit.
5. Wherever possible, surface parking should be avoided and below grade parking should be provided to ensure visual continuity and provide a positive pedestrian experience.
6. Where at-grade parking occurs it shall follow the requirement outlined in section 3.1.2 and be located behind buildings, away from the waterfront and have a landscape edge that moderates its visual impact from public view.



3.7.2 Beach Drive Built Form

Development along Beach Drive will have an uninterrupted view of the beach and water. Buildings along this edge will provide a defined built form edge that supports the public realm features of the boardwalk and pedestrian circulation along the waterfront.

1. Buildings located within the Beach Drive will be oriented in a way to maximize views of the water.
2. New development is encouraged to frame the views to Georgian Bay by orienting buildings towards the water and allow permeability of views between buildings.
3. Buildings shall be a minimum height of 3 storeys and a maximum height of 6 storeys. Where the height exceeds 3 storeys, a 3.0m stepback will be provided beginning at the 4th storey and will be applied for each additional storey above.
4. Raised terrace features / patios are encouraged along the Beach drive frontage to take advantage of views to Georgian Bay and to add articulation to the streetscape.
5. Raised terrace features shall provide visual interest in their design adjacent to the pedestrian clearway through articulation, material variation, and architectural detailing.

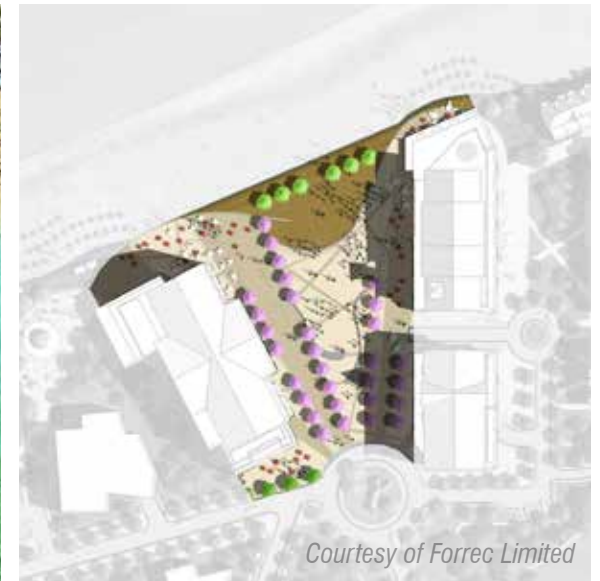


Courtesy of Forrec Limited

3.7.3 Festival Square Anchor

The Festival Square is envisioned as the major public open space for the Beach District. It is located at the end of Main Street and will anchor the Beach District while contributing to the link of destination spaces in the downtown. It will encourage pedestrian circulation along Main Street and terminate their trip at the waterfront

1. The buildings located in the Festival Square will consist of a mix of uses oriented to frame the square while drawing the flow of pedestrian movement to the beach front.
2. Buildings shall be a minimum height of 3 storeys and a maximum height of 6 storeys. Where the height exceeds 4 storeys, a 3.0m stepback will be provided beginning at the 5th storey and will be applied for each additional storey above.
3. Buildings will be oriented to enhance views to the waterfront with a continuous building wall that opens up towards the beach.
4. Buildings will be designed to support the public realm features and programme through active at-grade uses.
5. A variety of building types / uses are envisioned for the Square including hotel accommodation, entertainment venues, residential and retail/commercial uses.
6. Potential hotel uses should include at-grade services that engage the square, to provide a balanced level of retail activity and ensure active edges facing the square.



3.7.4 Resort/Residential Buildings

The Resort/Residential Zone is an area located in the westernmost end of the Beach District. It is envisioned to have privately owned seasonal accommodations that can be rented out year round when not in use by their owners. The residential nature of this type of development shall have regard for the guidelines noted in sections 3.4 Townhouses & Stacked Townhouses and 3.5 Low - Rise Apartments. In addition to these guidelines the following should be considered:

1. Low-rise residential townhouses that are 3 storeys in height shall provide a residential edge facing onto the beachfront walkway.
2. Low-rise condominium and apartment buildings with a heights of 4 to 6 storeys will be located behind them and will address the Mosley Street frontage.

3. Townhouses backing onto the interior low-rise condominium / apartment buildings shall have rear elevations with consistent window styles, architectural detailing and material cladding as the front elevation facing the beach.
4. Given the concentration of buildings in this area, amenity spaces and parkettes geared to this complex of building should be provided and be central to the site.
5. The buildings will be oriented to frame the public realm and street network and encourage grid-like network between buildings to allow for better pedestrian permeability
6. Parking is to be provided below grade for all residential uses in this area. Garage access at the rear of the townhouses may be considered where it is sensitively integrated into the townhouse unit design and provides an upgrade façade to the internal road.



Courtesy of Forrec Limited



3.8 Downtown Gateway District Built Form

Located at the southeast end of Main Street, the Upper Main District is envisioned as a transition area to the less urban built form uses along River Road West. In addition to the general guidelines outlined in 3.5 Guidelines for Mixed-Use Buildings, the following shall be considered for buildings in the Downtown Gateway District:

3.8.1 General Guidelines for the Downtown Gateway District:

1. Buildings shall be a minimum height of 3 storeys and a maximum height of 6 storeys. Where the height exceeds 3 storeys, a 3.0m stepback will be provided beginning at the 4th storey and will be applied for each additional storey above.
2. Although 100% building frontage is encouraged for properties in this district, given the existing larger scale retail uses, frontages of 75% to 50% are permitted. To create a better pedestrian experience on Main Street, building facades should extend as close to 100% of the property frontage, as possible.
3. Where new buildings are located next to properties with parking forecourts, the side elevations of the new buildings should include material detailing, active windows with clear glazing where possible, and/or other architectural features to address street views of the flanking views.
4. All new buildings shall be sited and located at the property line to reinforce the pedestrian street edge of Main Street.
5. Buildings that do not extend the full width of the property frontage, should contribute to a continuous street edge by providing soft and/or hard landscape features to define the rest of the frontage and mitigate visual impact of parking areas.
6. Auto-oriented uses such as drive-throughs shall not be permitted along Main Street or its associated side streets in the Downtown Gateway District.
7. Service areas including loading and garbage, shall be located in locations that are not directly visible to Main Street and/or provide a screening wall integrated into the building design.
8. Where possible, service areas should be integrated and shared between two adjacent buildings.
9. Architectural screening or enclosures of service areas should be built with materials/colours complementary to the building style.



3.8.2 Gateway Buildings

The gateway is located at the intersection of Main Street and River Road West and is the main access point into the downtown. This area is key in creating a first impression to draw residents and visitors into the area. To promote a recognition of a sense of entry into the downtown, gateway treatments such as more prominent building design and integrated landscaping are encouraged.

1. Buildings located at the gateway will have massing that addresses the corners and roundabout feature, through additional building height at the corners of the block, to accentuate the importance of these locations.
2. Gateway buildings should be oriented to address the street and clearly defined entrances should be provided from the Main Street sidewalk.
3. Buildings should be a minimum height of 3 storeys and a maximum height of 6 storeys. Building facades should be articulated through wall

plane changes and extensive fenestration to address public views.

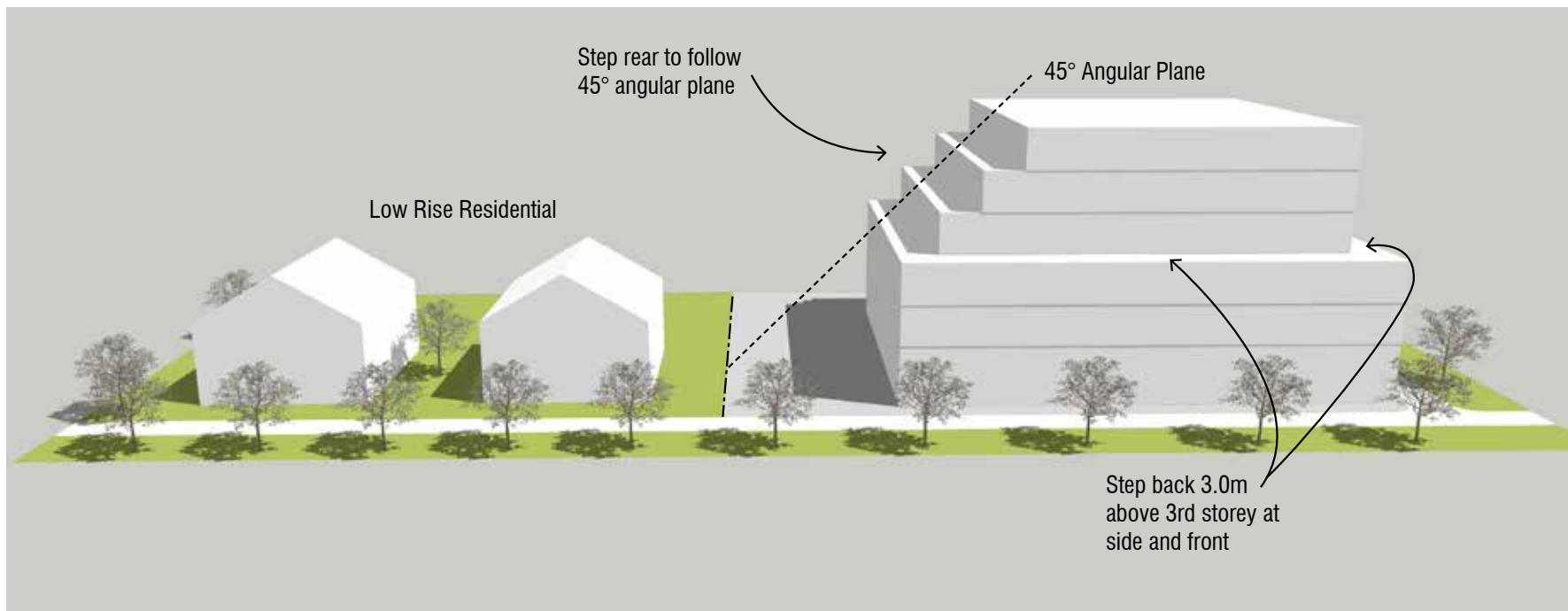
4. Added height from architectural features will be permitted where they contribute to the sense of entry at the gateway.
5. Gateway buildings shall have consistent architectural detail treatment and features, window styles, and cladding materials on all 4 elevations.
6. Where possible, integrate Wasaga Beach gateway signage into the design of the gateway building.



3.9 Transition Guidelines to Adjacent / Existing Residential Uses in Neighbourhoods

The focus on the development of the downtown, will also require sensitive consideration of how that development transitions to existing residential uses in supporting adjacent neighbourhoods. The following guidelines should be taken into consideration:

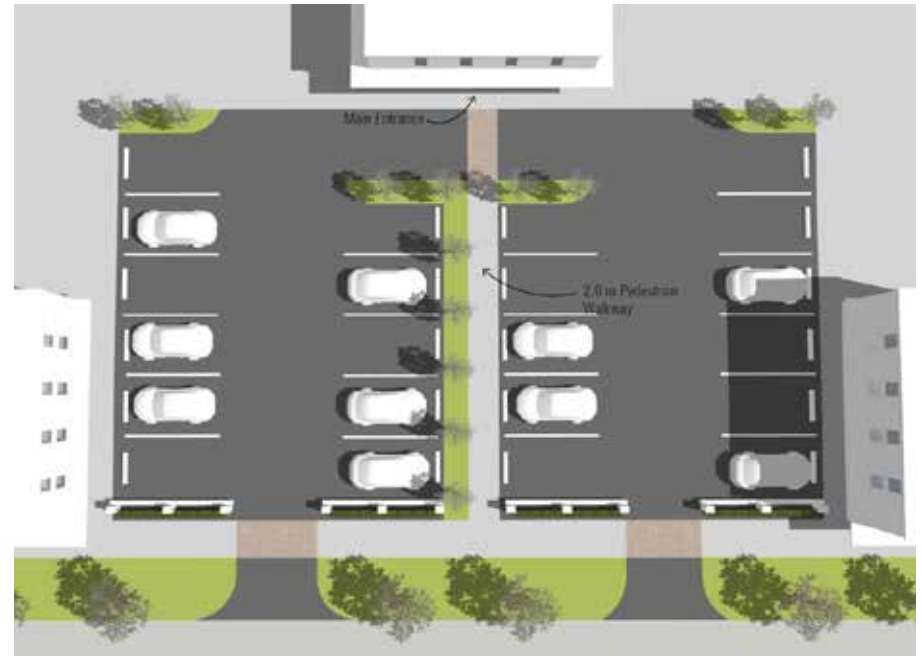
1. The side and rear of buildings abutting low-rise residential properties shall generally be of similar height as the residential dwellings or should be stepped to maintain an appropriate scale and transition in relation to adjacent residential uses.
2. To provide sensitive transition of mixed-use built form massing and heights, where they abut low-rise residential buildings, a building should be designed to fit within a 45-degree angular plane which is applied and measured from the abutting property line.
3. The maximum variation in the transition of height should be a one storey height to allow for progressive height changes to adjacent uses.
4. Rear elevation setbacks from Main Street buildings should be a minimum 7.5m.
5. A privacy fence and a minimum 1.5m planted buffer along abutting property lines should be provided within the Main Street building property.



Example of transitioning to low rise residential

3.10 Private Roads, Lanes, Midblock Connections

1. As part of a complex of buildings, private roads may be permitted provided they have similar characteristics to a typical local road (minimum width of 13.5m including parking laybys) and must also ensure the following:
 - where access to below grade parking is not possible a private road is required to access units or shared at-grade parking areas;
 - buildings face onto and address the street;
 - pedestrian walkways are provided and demarcated so as to provide safe pedestrian circulation;
 - where roads are a shared space, special paving shall be used to differentiate pedestrian and vehicular zones;
 - incorporates traffic calming features and,
 - landscaping and tree planting shall be included as part of the private road streetscape.
2. Lanes are encouraged mid-block from a side street to provide access to parking areas behind the Main Street buildings, below grade parking areas, and parking in transitional residential areas. They shall include the following:
 - the lanes should be between 8.5m (two travel lanes and a 2.0m walkway) and 11.5m (two travel lanes, 2.5-3.0m planted edge and a 2.0m walkway);
 - snow storage areas will be incorporated into the lane;
 - permeable paving materials shall be encouraged in lane areas to assist in drainage and stormwater management;
 - connectivity to another road or secondary access from the side street; and,
 - landscaped islands integrated with the built form rear yards or with the at-grade parking areas behind buildings.



Defined pedestrian walkway

3. Mid-block pedestrian connections are encouraged where there are long blocks present and to enhance the pedestrian network within the downtown. They are typically found within the private realm but they connect public realm circulation. They should include the following:

- provide flexibility to accommodate pedestrians and cyclists and can include walkway widths that assist emergency services in the town;
- where linked to Main Street may also incorporate a width that could accommodate outdoor patio areas as well a pedestrian clearway to add more vibrancy to these paths;
- where possible they should include weather protection from canopies on adjacent buildings;

- special paving could be extended and provide a defined pedestrian route to another street or internal parking area;
- where possible, they should accommodate streetscape furniture and bicycle parking to support active transportation in the Downtown;
- there should be active fenestration on the flanking building to assure passive surveillance and overlook of the connection; and,
- they shall be well lit and include lighting that is pedestrian appropriate throughout the day.



Example mid-block connection